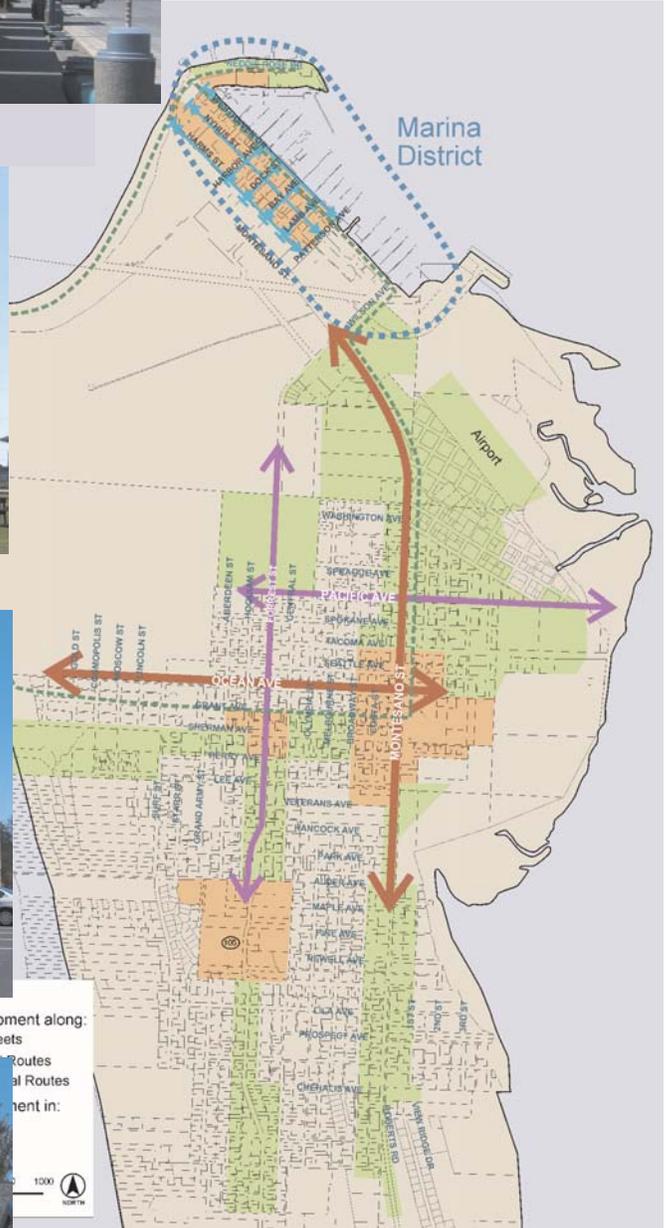
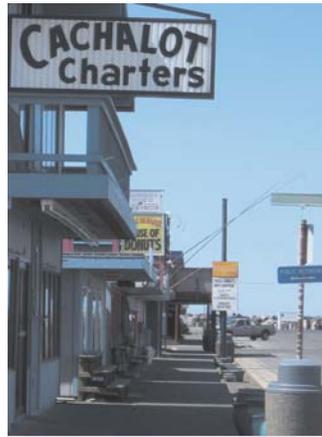


# CITY OF WESTPORT

## Design Standards & Guidelines



Prepared by  
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# Westport Design Standards and Guidelines

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# Introduction

## Purpose

Design guidelines and standards are critical tools in guiding private development in a way that realizes the community’s goals and objectives. These design guidelines and standards are intended to:

- Provide clear objectives for those embarking on the planning and design of projects in Westport’s designated commercial zones.
- Add consistency and predictability to the permit review process.
- Cultivate a community identity, create an image and coordinate the visual aesthetic.
- Promote economic vitality.

## What properties do the standards and guidelines apply to?

The standards and guidelines herein apply to all **non-single family development** on the properties specifically noted in each standard & guideline; the areas where the guidelines & standards may apply include:

- Properties in MUTC 1 and MUTC 2 zones in the Marina District (see Figure 1).
- Properties along *pedestrian streets* in MUTC 1 and MUTC 2 zones in the Marina District (see Figure 1). *Pedestrian streets* are streets that evoke a “main street” feel, and include the following:
  - Wide sidewalks
  - Street parking
  - Buildings close to the sidewalk
  - Pedestrian amenities (pedestrian lighting, street furniture, e.g.)



Figure 1. In the Marina District, development along pedestrian streets should promote a “main street” feel.

In Westport, the streets that are classified as *pedestrian streets* are Westhaven Drive, Dock Street, Nyhus Street, Harms Street, Lamb Avenue, Bay Avenue, and Harbor Avenue.

- Properties in MUTC 1 and MUTC 2 zones which are directly adjacent to the *primary arterial routes* and *secondary arterial routes* identified in Figure 2. *Primary arterial routes* are vehicular access routes that act as main arterials, usually with medium to large scale development and less pedestrian activity. *Secondary arterial routes* are vehicular access routes with less activity and smaller-scale commercial development than *primary arterial routes*. In Westport, the streets that are currently classified as *primary arterial routes* are Montesano Street and Ocean Avenue. *Secondary arterial routes* include only Pacific Avenue and Forrest Street.

## When do I need to comply?

The design standards and guidelines apply to all new **non-single family residential** development and *major exterior remodels*. However, setting requirements for proposed exterior remodels presents an interesting problem. On the one hand, Westport would benefit greatly if all the standards and guidelines were met when properties are significantly improved. On the other hand, no one wants to disadvantage property owners or discourage them from improving their buildings. The recommended solution is to establish two thresholds to gauge the extent of remodeling and set requirements based on what is practical and reasonable for that level of improvement.

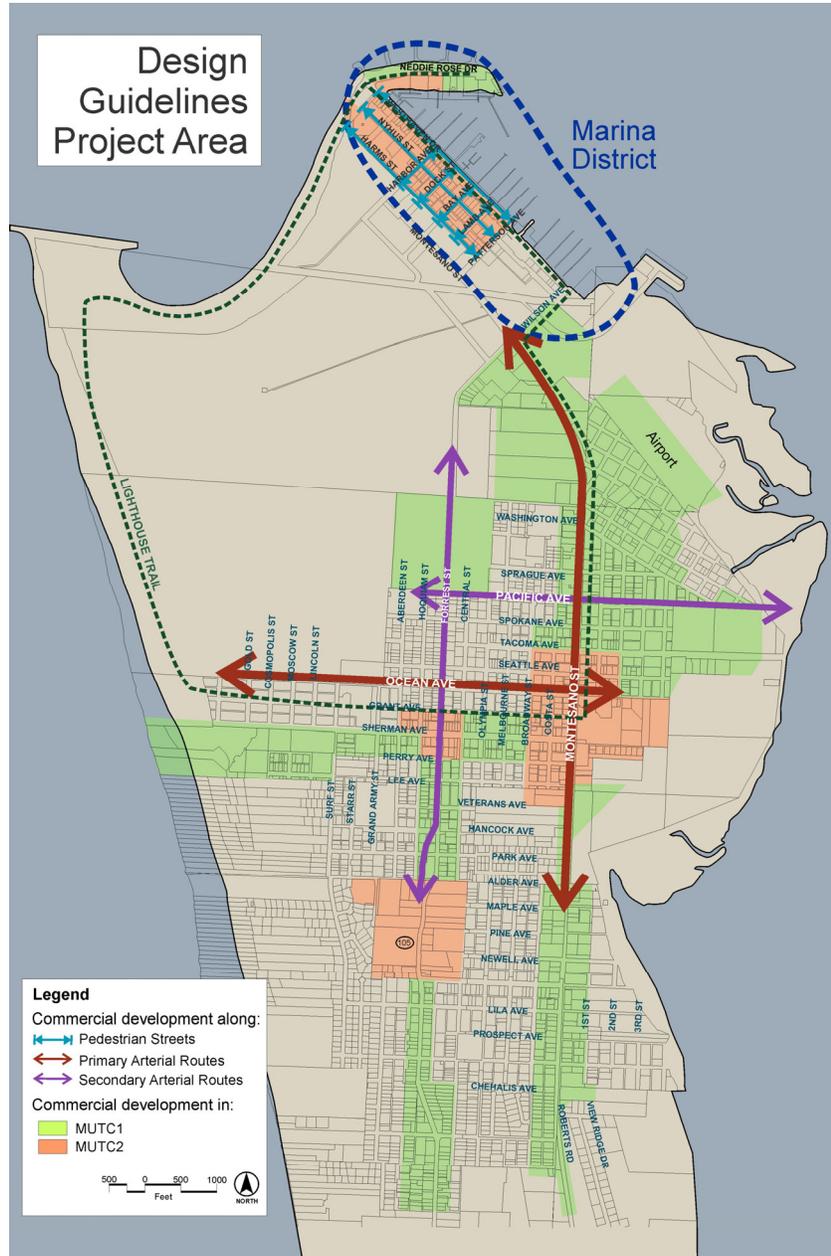


Figure 2. All streets that classify as pedestrian streets, primary or secondary arterial routes are included in the above illustration. As Westport grows, additional streets may be included.

*Major exterior remodels* include all remodels within a three year period whose value exceeds 50 percent of the value of the existing structure, as determined by the City of Westport valuation methods. All standards and guidelines that do not involve repositioning the building or reconfiguring site development shall apply to *major exterior remodels*.

For exterior remodels within a three year period with value of 50 percent of the building valuation or less (*minor exterior remodels*), the requirement is only that the proposed **improvements** meet the standards and/or guidelines and do not lead to further nonconformance with the standards and guidelines. For example, if a property owner decides to replace a building façade's siding, then the siding shall meet the applicable exterior building material and color standards and/or guidelines, but elements such as building modulation would **not** be required.

The standards and guidelines do not apply to remodels that do not change the exterior appearance of the building.

## How does the process work?

These standards and guidelines should be studied at the beginning of a prospective applicant's planning process and are intended to make people aware of the design issues that warrant early consideration. It is recommended that prospective applicants engage in a "Pre-Application Review" meeting with City / Planning staff prior to the submission of a building permit application and relevant zoning permits. The goal of this meeting is to provide clear direction to the applicant early in the process, provide for an informal discussion of site specific design issues and opportunities, and minimize the need for design changes late in the design phase.

The Design Standards and Guidelines shall serve as a supplement to Chapter 17.20A, Section 060 (Development Standards) in the Westport Municipal Code (WMC). Where there is a conflict between the Design Standards and Guidelines and Chapter 17.20A, the more specific regulations shall apply as determined by the City.

All permit applications are reviewed by City / Planning staff to determine if the application complies with the Standards and Guidelines. Appeals are referred to the Planning Commission.

## How are the design standards and guidelines applied?

Each chapter of the Design Standards and Guidelines contains a list of "Intent" statements followed by "Standards" and/or "Guidelines." Specifically:

- **Intent** statements are overarching objectives. For example, one of the Intent statements for the sub-chapter on Building Location and Orientation is to "create an active and safe pedestrian environment."
- **Standards** use words such as "shall", "must", "is/are required", or "is/are prohibited" and signify **required** actions. In special circumstances, the City will allow alternative design treatments as long as applicants can demonstrate that the proposed design/feature meets the Intent.
- **Guidelines** use words such as "should" or "is/are recommended" to signify **voluntary** measures which are encouraged or discouraged but not required or prohibited.

## Departures

The City may allow departures from required standards in the following circumstances:

- Where conformance with a standard conflicts with another standard, goal or policy as determined by City / Planning staff.
- Where *unique natural features* prevent an applicant from conforming to a standard, as determined by City / Planning staff.
- Where a unique lot shape or small lot size make it extremely difficult to conform to a standard, City / Planning staff may eliminate or reduce the requirement.
- In each case above, the applicant must utilize other methods per City/Planning staff's satisfaction that meet the Intent of the applicable standard or standards.

## Organization

The design standards and guidelines are organized into four primary chapters, each with several sub-chapters that include Intent statements followed by specific guidelines and/or standards, as outlined in the Table of Contents. Some topics are addressed in more than one section. For example, parking lot landscaping requirements are found in both the Parking Area and Driveway Location and Design section of Chapter 1 (Site Planning), and in the Landscaping chapter (Chapter 4).

## Definitions

Words within the standards and guidelines that are *italicized* are defined in Chapter 5.

# 1. Site Planning

## 1.1. Building Location and Orientation

### Intent

- To encourage development in the Marina District that creates an active and safe pedestrian environment.
- To reinforce Westport's pattern of storefronts along Westhaven in the Marina District.
- To improve the pedestrian environment along arterials.
- To upgrade Westport's visual identity.

### Design Standards

- 1.1.1. Buildings along *pedestrian streets* are required to have the primary entrance facing and built to the sidewalk's edge.

**Exceptions to 1.1.1:** Setbacks are allowed along *pedestrian streets* if the space between the sidewalk and the building is maintained as *pedestrian-oriented space*. To qualify as *pedestrian-oriented space*, the following must be included:

- Visual and pedestrian access (including barrier-free access) to the abutting structures from the public right-of-way or a non-vehicular courtyard.
- Paved walking surfaces of either concrete or other approved unit paving.
- Pedestrian-scaled lighting (no more than 14' in height) at a level averaging 2-foot candles throughout the space. Lighting may be on-site or building-mounted lighting.
- At least two feet of seating area (bench, ledge, etc.) or one individual seat per 60 square feet of plaza area or open space.

The following features are encouraged within pedestrian-oriented space:

- Pedestrian amenities such as a water feature, drinking fountain, tables, and/or distinctive paving or artwork.

The following features are discouraged within *pedestrian-oriented space*:

- Asphalt or gravel pavement.
- Adjacent unscreened parking lots.
- Adjacent chain link fences.
- Adjacent blank walls.
- Adjacent dumpsters or service areas.

1.1.2 Buildings along *pedestrian streets* must have *pedestrian-oriented facades*, which include transparency (windows / doors) covering at least 75% of the ground floor *façade* (between 2 and 8 feet), and three of the following elements:

- *Weather protection* (awning, marquee, canopy, e.g. that is at least 8' above the walking surface and projects at least 4-1/2 feet from the structure)
- Wall mounted signage (see 3.3.6 for signage requirements)
- Wall mounted lighting
- Hanging/potted plants
- Other wall mounted decorative elements that meet the Intent (subject to approval)

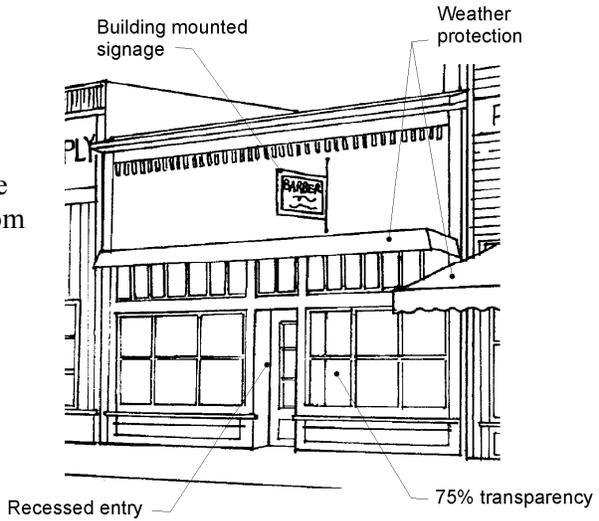


Figure 3. Example of a pedestrian-oriented façade.

1.1.3. Development along *primary arterial routes* must be setback at least 10' between the sidewalk's edge (the edge adjacent to the lot) and the building or parking area and must provide *landscaping or pedestrian-oriented space* in the setback area.

1.1.4 Development along *secondary arterial routes* must be setback at least 5' between the sidewalk's edge (adjacent to the lot) and the building or parking area and must provide *landscaping or pedestrian-oriented space* in the setback area.



Figure 4. Facades along pedestrian streets can include setbacks for pedestrian-oriented space, which helps to encourage activity on the street.

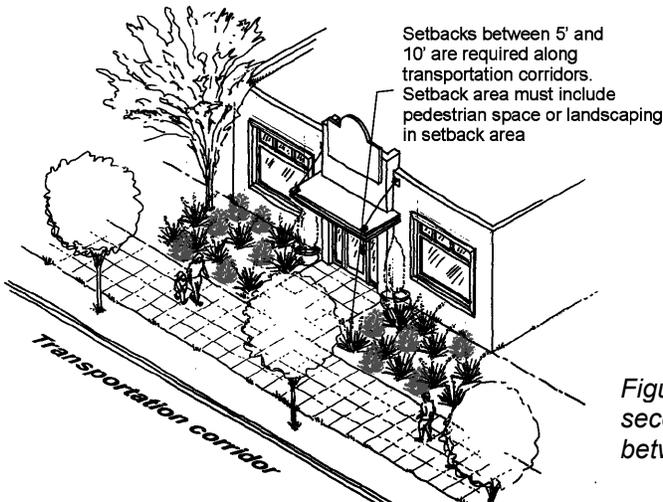


Figure 5. Setbacks along primary and secondary arterial routes provide a buffer between vehicles and building entries.

## 1.2 Service, Loading, Outdoor Storage and Mechanical Areas

### Intent

- Minimize adverse visual, olfactory, or auditory impacts of service, loading, outdoor storage and mechanical equipment areas at ground and roof levels.
- Encourage more thoughtful siting and reduce impacts of service, outdoor storage and mechanical areas.

### Standards

- 1.2.1. **All Sites:** Service, storage, loading and mechanical areas, utility meters, electrical and other utility apparatus shall be located to reduce adverse sensory impacts (visibility, noise, smell).
- 1.2.2. On *pedestrian streets*, roof-mounted mechanical equipment should be located so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. If visual impact is unavoidable, roof-mounted equipment should be screened with materials that blend with the architectural character of the building and color of the roof.
- 1.2.3. On *pedestrian streets*, service, storage, loading and mechanical areas, utility meters, electrical and other utility apparatus located at ground level shall be *screened* and attached to structures to reduce visual impacts from *streets* and *adjoining* properties. Screening features should use materials and forms that blend with the architectural character of the building.

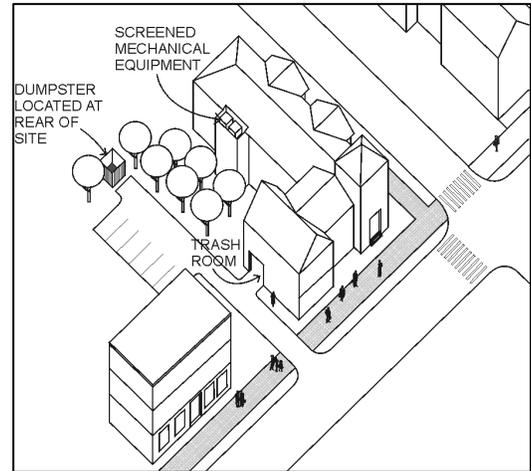


Figure 6. Locate service areas to minimize impacts on the pedestrian environment.



Figure 7. Where service elements are visible to the public, they should be screened. This example includes landscaping and masonry.

### Guidelines

- 1.2.6. On *primary* and *secondary arterial routes*, *landscaping* or other forms of *screening* around outdoor service, storage, loading and mechanical areas, utility meters, electrical conduit, and other service and utilities apparatus is encouraged.

## 1.3 Parking Area and Driveway Location and Design

### Intent

- Provide safe, convenient access to and within sites.
- Minimize visual impacts of parking facilities, particularly along pedestrian streets.
- Minimize driveway impacts across pedestrian walks.

### Design Standards

- 1.3.1. On *pedestrian streets*, parking or open storage of vehicles must be located to the side or rear of buildings (see Figure 8).
- 1.3.2. On *primary and secondary arterial routes*, single row pull-in parking or open storage of vehicles between the street and the building must be limited to 60 feet (measured along the street) in length between the street and structure (see Figure 9). All off-street parking lots or open storage of vehicles (lots with more than a single row of pull-in parking) may not exceed 50% of the lot frontage between the street and structure (see Figure 10).

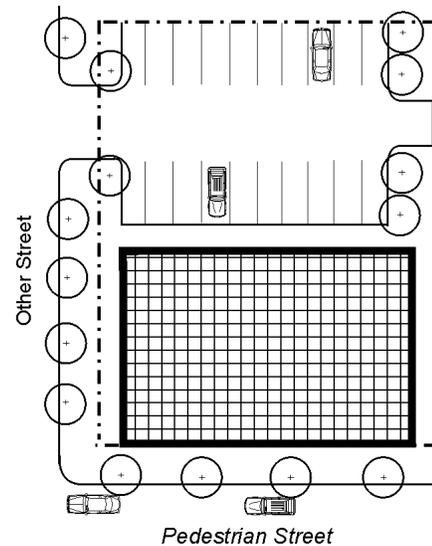


Figure 8. Along pedestrian streets, all parking facilities provided on site must be located at the side or rear of the building.

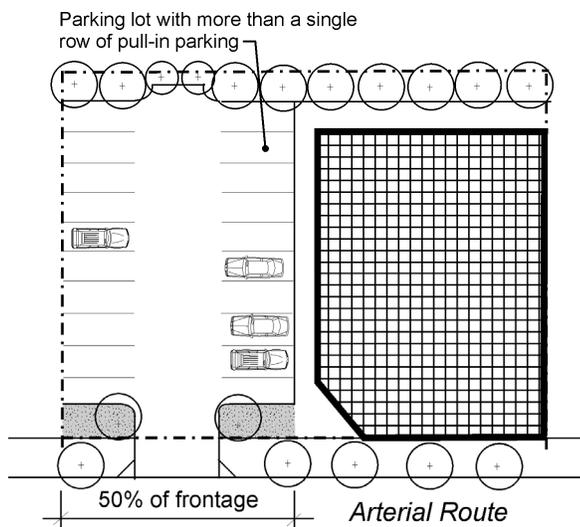


Figure 10. Lots consisting of more than a single row of pull-in parking may not occupy more than 50% of the lot frontage along the arterial route.

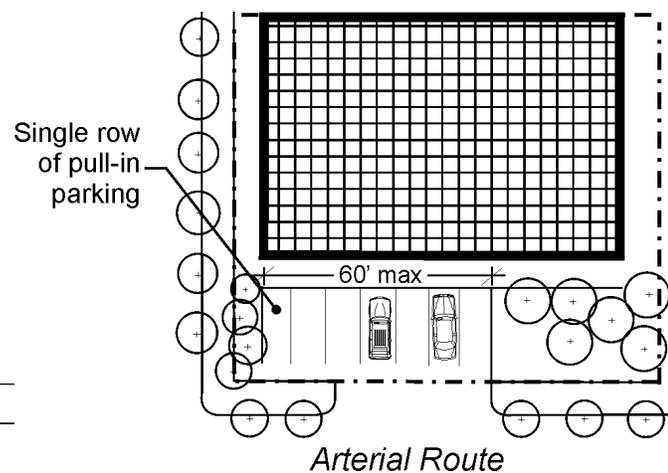


Figure 9. Along arterial routes, 60' of single-row pull-in parking may be located between the building and the street.

1.3.3. Along *primary arterial routes*, new parking lots accommodating more than 14 stalls adjacent to any street must incorporate the following design treatments to reduce the visual impact of the parking lot on the street:

- Landscaped planting strip at least ten feet in width (consistent with the setback requirement outlined in Standard 1.1.2), measured from the sidewalk's edge. Standards for planting shall be as follows:
  - Trees at an average minimum rate of one tree per 30 lineal feet of street frontage.
  - Shrubs at the minimum rate of one per 20 square feet of landscaped area. Shrubs should have a mature height between three and four feet.
  - Groundcover shall be planted and maintained in sufficient quantities to provide at least 85 percent coverage of the landscaped area within three years of installation.

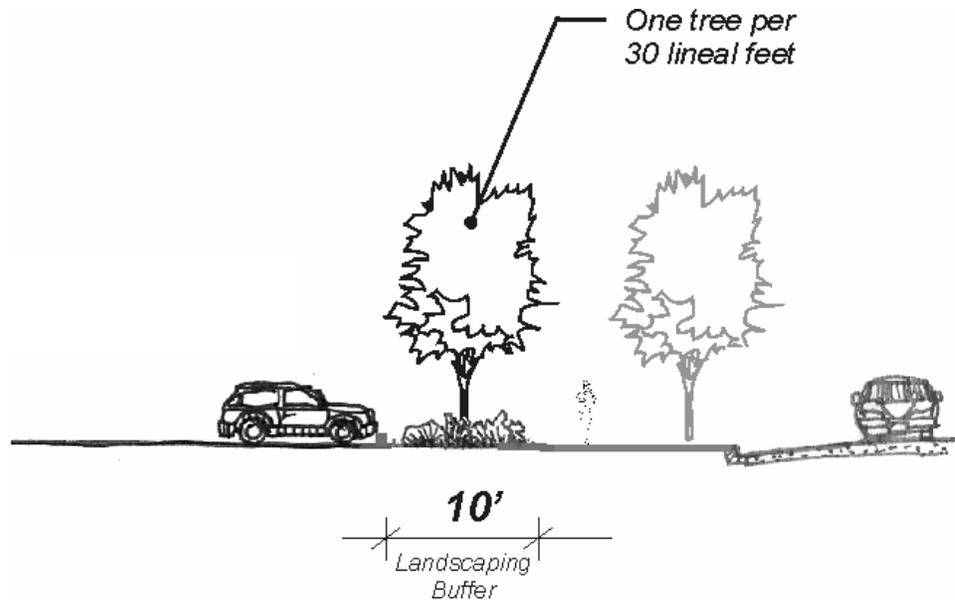


Figure 11. Landscaped planting strips reduce the visual impact of parking areas on the street and pedestrian environment.

1.3.4. Along *secondary arterial routes*, the planting strip is reduced to 5 feet (consistent with the setback requirement outlined in Standard 1.1.3) in width, measured from the sidewalk's edge; the above planting standards (Standard 1.3.3) apply.

1.3.5. New parking lots accommodating more than 14 stalls located in the MUTC zones of the Marina District adjacent to any street must incorporate at least one of the following design treatments to add seasonal interest and reduce the visual impact of the parking lot:

- Five foot planting strip including the planting requirements described in Standard 1.3.3.
- A trellis or other similar architectural element that incorporates landscaping.
- A decorative *screen* wall or low wall (Figure 12) that incorporates landscaping sufficient to mitigate the visual impact of the parking area on the street.



Figure 12. A screen wall example that incorporates landscaping.

Note: For security, all screening treatments must maintain maximum visibility at eye level between parking area and sidewalk.

### Design Guidelines

- 1.3.7. Along *primary* and *secondary arterial routes*, locating off-street parking areas to the rear or side of buildings is encouraged.
- 1.3.8. **All sites:** Developments should break up large parking lots into smaller areas to the extent reasonably possible.
- 1.3.9. **All sites:** Parking lots located adjacent to intersections are discouraged.

## 2. Pedestrian Access & Amenities

### 2.1 Sidewalks and Pathways

#### Intent

- To enrich the pedestrian environment and encourage pedestrian activity, particularly in the Marina District.
- To improve the pedestrian environment along *arterial routes*.
- To enhance the quality of new developments.

#### Design Standards

- 2.1.1 *Pedestrian streets* with an 80-foot Right of Way (R.O.W.) or greater require a sidewalk with at least 8 feet of unobstructed width.

*Exception:* Due to the existing R.O.W. and parking pattern (angled) along Westhaven Drive, new development and redevelopment along Westhaven Drive must retain existing sidewalk width, but is exempt from the 8-foot minimum requirement. **If in the future Westhaven is reconfigured to accommodate one-way traffic flow, the City may reconsider this exemption.**

- 2.1.2 *Pedestrian streets* with less than 80-foot R.O.Ws require sidewalks with at least 6 feet of unobstructed width. Where rights-of-way are insufficient to provide the required widths, buildings shall be setback to meet sidewalk requirements.
- 2.1.3 *Primary arterial routes* with an 80-foot Right of Way (R.O.W.) or greater require sidewalks with at least 6 feet of unobstructed width.

Street trees and/or pedestrian street lamps every 30' are encouraged

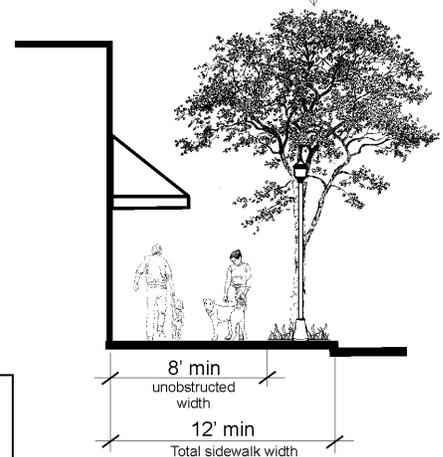


Figure 13. 80' R.O.W. pedestrian street requirements.

Street trees and/or pedestrian lighting desirable

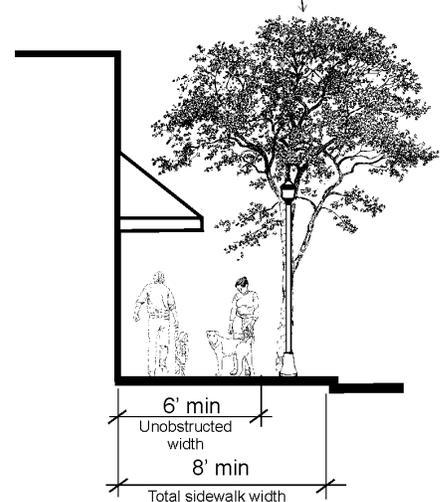


Figure 14. 60' R.O.W. pedestrian street requirements.

- 2.1.4 *Secondary arterial routes* with a 60-foot Right of Way (R.O.W.) or greater require sidewalks with at least 5 feet of unobstructed width.

*Exception:* Where alternate width is designated in Westport’s Corridor Enhancement Design Plan, which addresses design of Montesano between Dock and Ocean, and Ocean between Montesano and Forrest

**Design Guidelines**

- 2.1.5 Along *pedestrian streets*, street trees every 25’ and pedestrian scaled lighting every 50’ is encouraged.
- 2.1.5. Along *arterial routes* pedestrian scaled lighting every 50’ is desirable.



Figure 15. Per the Corridor Enhancement Design Plan the existing sidewalk along Montesano at Elizabeth Street is 8’ (top); the sidewalk along Montesano west of Wilson Avenue is 6’.

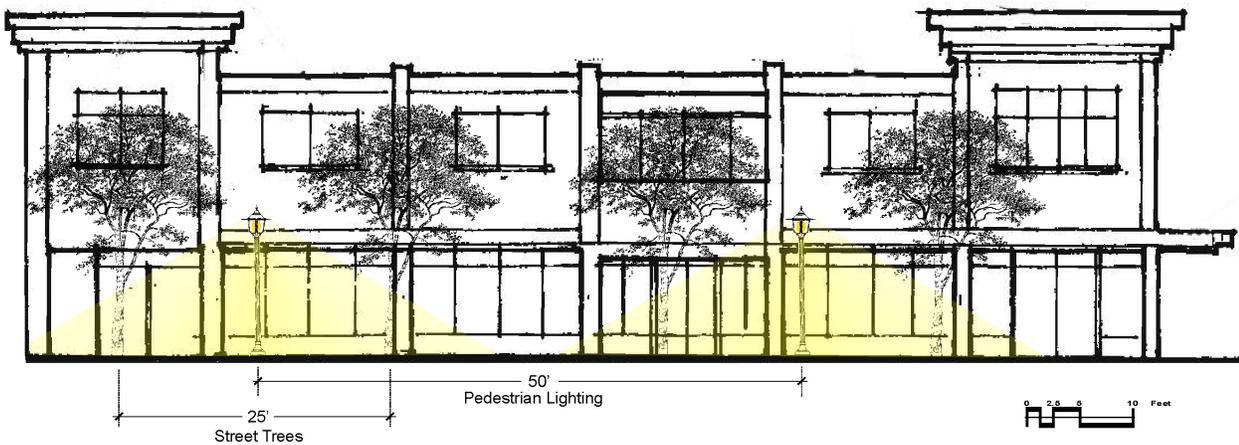


Figure 16. Evenly spaced street trees and pedestrian-scaled lighting enhances the pedestrian environment and encourages evening activity.

## 2.2 Secondary Entrances

### Design Standards

- 2.2.1 Secondary entrances to buildings along *pedestrian streets* must incorporate at least **two pedestrian-oriented façade** or *pedestrian-oriented space* improvement elements.

*Pedestrian façade* elements include:

- Transparency (windows / doors) covering at least 75% of the ground floor *façade* (between 2 and 8 feet)
- *Weather protection* (awnings, e.g.) To qualify as weather protection, the feature must be at least 8' above the walking surface and project at least 4-1/2 feet horizontally from the structure.
- Wall mounted signage and lighting
- Other wall mounted decorative elements that meet the Intent

*Pedestrian-oriented spaces* can include:

- Street-furniture
- Pedestrian lighting
- Decorative paving
- Landscaping or potted plants/planters
- Other decorative outdoor features that meet the Intent (fountains, sculptures, statues, etc.)

### Design Guidelines

- 2.2.2 Secondary entrances along *primary transportation routes* are encouraged to include at least **two pedestrian-oriented façade** or *pedestrian-oriented space* improvement elements see Standard 2.2.1).



Figure 17. Providing pedestrian amenities at secondary entrances off parking lots or alleys can greatly improve visual appeal and increase activity at the rear of buildings.

## 2.3 Pedestrian-Oriented Space

### Design Guidelines

2.3.1 *Large-scale developments* (sites two or more acres) are encouraged to incorporate *pedestrian-oriented space* into the design of the site. As stated in Chapter One, to qualify as *pedestrian-oriented space*, an area must include the following:

- Visual and pedestrian access (including barrier-free access) to the abutting structures from the public right-of-way or a non-vehicular *courtyard*.
- Paved walking surfaces of either concrete or other approved unit paving.
- Pedestrian-scaled lighting (no more than 14' in height) at a level averaging 2-foot candles throughout the space. Lighting may be on-site or building-mounted.
- At least two feet of seating area (bench, ledge, etc.) or one individual seat per 60 square feet of plaza area or open space.

The following features are encouraged within pedestrian-oriented space:

- Pedestrian amenities such as a water feature, drinking fountain, tables, and/or distinctive paving or artwork.

The following features are discouraged within *pedestrian-oriented space*:

- Asphalt or gravel pavement.
- Adjacent unscreened parking lots.
- Adjacent chain link fences.
- Adjacent blank walls.
- Adjacent dumpsters or service areas.



*Figure 18. Pedestrian-oriented space encourages pedestrian activity and improves the overall pedestrian environment.*

# 3. Building Design

## 3.1 Architectural Style/Character

### Intent

- To reinforce the unique character of Westport.
- To encourage developments to employ desirable architectural features found in historical Westport buildings without promoting a false sense of historicism.

### Standards

3.1.1. Along *pedestrian streets*, architectural character based on a traditional maritime theme is required; applicants must select three of the following features:

- Horizontal or shingle siding
- Pitched or *gabled roof*
- Transom windows
- Architectural details such as dormers or a cupola
- Thematic details such as lighting, railing, weathervane, appropriately designed columns
- Other architectural features that meet the Intent (subject to approval)

However, applicants are discouraged from employing these components in a manner that evokes a false sense of historicism (i.e. when buildings are designed precisely to look aged or old). Rather, **applicants are encouraged to consider modern interpretations of these building elements and components** in a way that complements the surrounding context.

- ① Pitched roof
- ② Cupola
- ③ Dormer
- ④ Transom windows
- ⑤ Horizontal or shingle siding
- ⑥ Thematic details



Figure 19. The Marina District's Maritime Museum (above) embodies Westport's traditional maritime-themed architecture; several new developments in Westport have chosen to incorporate the same thematic elements.

3.1.2. In the Marina District, architecture that is defined predominately by corporate identity features (and difficult to adapt to other uses) is prohibited. For example, some fast food franchises have very specific architectural features that reinforce their identity. Besides diluting the town's identity with corporate (and therefore generic) identities these buildings are undesirable because they are not adaptable to other uses if the corporate franchises leave.



Figure 20. Several northwest communities, including Gig Harbor (above), have prohibited large corporate identity features.

### Guidelines

3.1.3. Along *primary* and *secondary arterial routes*, architecture that is defined predominately by corporate identity features (and difficult to adapt to other uses) is discouraged.

## 3.2 Architectural Scale and Building Mass

### Intent

- To ensure the bulk and mass of buildings are compatible with Westport's scale, particularly in the Marina District.
- To add visual interest to buildings.
- To enhance the character and identity of Westport.

### Design Standards

- 3.2.1. Buildings facing *pedestrian streets* must include modulation and/or articulation features every 50 feet to protect the pattern of small storefronts in the area. At least three of the following methods must be employed:
- Use of window and/or entries that reinforce the pattern of 50-foot commercial spaces.
  - Use of *weather protection* features that reinforce 50-foot commercial space (must be at least 8' above the walking surface and project at least 4-1/2 feet from the structure).
  - For example, for a business that occupies three lots, use three separate awnings to break down the scale of the storefronts.
  - Change of roofline.
  - Change in building material or siding style.
  - Other methods that meet the Intent of the standards (subject to approval).

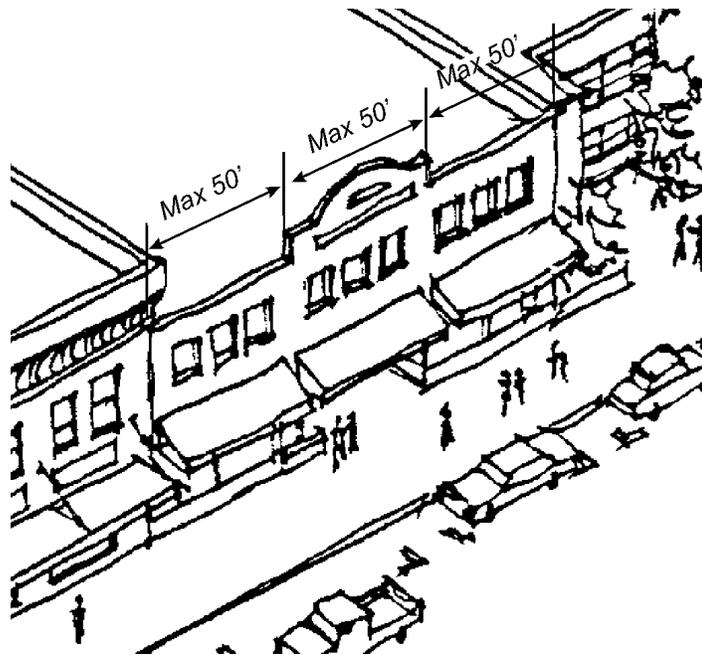


Figure 21. This building utilizes fenestration patterns of windows and awnings and roofline modulation to reduce its architectural scale and add visual interest.

3.2.2. The maximum *façade* width (the *façade* includes the apparent width of the structure facing the street) of the upper story of multi-story *buildings* visible from a street, public open space, or *pedestrian-oriented space* is 120 feet. Buildings exceeding 120 feet in width shall be divided by a 30-foot wide *modulation* of the exterior wall, so that the maximum length of a particular *façade* is 120 feet. Such *modulation* must be at least 20 feet deep and extend through all floors except the ground floor where a *pedestrian-oriented façade* has been provided. Decks and roof overhangs may encroach up to 3 feet (per side) into the *modulation*.

**Exceptions:** City / Planning staff will consider other design methods that are effective at reducing the perceived width of the building. An example of an alternative design that succeeds in reducing the perceived width of a building is included at the bottom right of Figure 22.

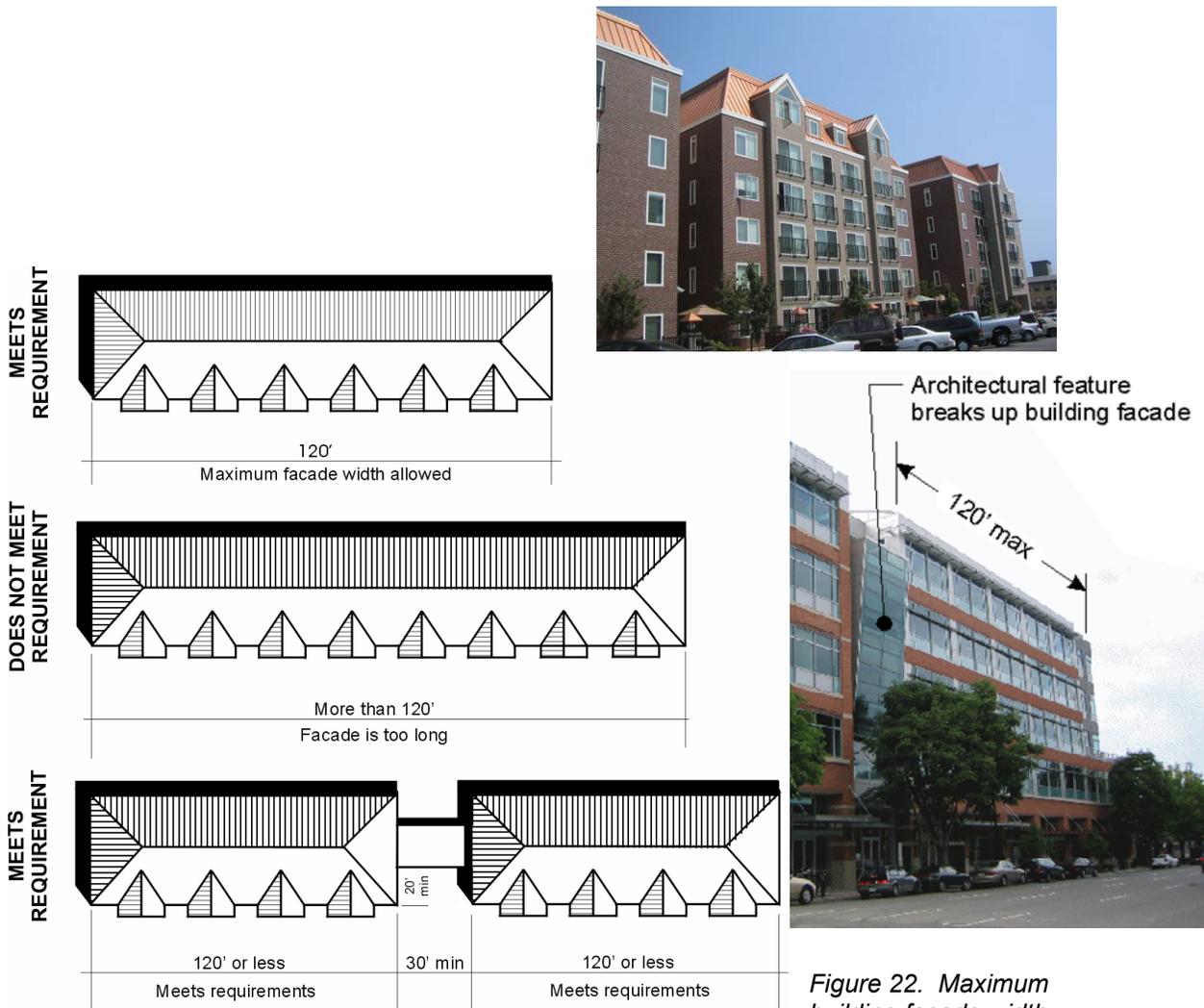


Figure 22. Maximum building façade width standards and examples

This buildings exceeding 120 feet in width along the street front, but is divided by a 30-foot wide *modulation* of the exterior wall, so that the maximum length of a particular *façade* is 120 feet or less. Such *modulation* must be at least 20 feet or deeper and extend through all floors.

## Design Guidelines

- 3.2.3. Buildings along *primary arterial routes* are encouraged to provide at least two scaling techniques (described in Standard 3.2.1) every 75 feet.
- 3.2.4. Buildings along *secondary arterial routes* are encouraged to include at least two scaling techniques (described in Standard 3.2.1) every 50 feet.
- 3.2.5. In the Marina District, buildings with three or more stories are encouraged to moderate vertical *scale* by including techniques to clearly define the building's top, middle and bottom. The following techniques are suggested methods of achieving vertical *articulation*:
- Top: Sloped roofs, strong eave lines, *cornice* treatments, horizontal trellises or sunshades, etc.
  - Middle: Windows, *balconies*, material changes, railings and similar treatments that unify the *building* design.
  - Bottom: Pedestrian-oriented storefronts, *human scale* building details, awnings, and arcades.



Figure 23. Utilize design techniques to clearly define a multi-story building's top, middle, and bottom.

### 3.3 Building Details and Materials

#### Intent

- To encourage the incorporation of design details and small scale elements into building facades that are attractive at a *human scale* in the Marina District.
- To encourage creativity in the design of building facades to add visual interest on all sites.
- To encourage high quality building materials that will upgrade the character and identity of Westport.
- To discourage materials and colors that are not compatible with the character and scale of Westport.
- To discourage materials and treatments of buildings that create a false sense of historicism or theme in new development.

#### Design Standards

3.3.1 Untreated *Blank walls* visible from a *pedestrian street* are prohibited. Methods to treat *blank walls* can include:

- Transparent windows or doors.
- Display windows.
- Landscape planting bed at least five feet wide or a raised planter bed at least two feet high and three feet wide in front of the wall with planting materials sufficient to obscure or screen at least 35 percent of the wall's surface within three years.
- Installing a vertical trellis in front of the wall with climbing vines or plant materials.
- Painted murals / artwork.
- Other approved methods that meet the Intent.



Figure 24. An example of landscaping that successfully mitigates the impact of a blank wall on the adjacent sidewalk.

3.3.2 In the Marina District, the following building materials are limited to 15% of the *façade*:

- Plywood siding
- Tinted / mirrored glass
- Corrugated fiberglass
- Non-corrugated, reflective sheet metal

3.3.3 Facades along *pedestrian streets* must limit the number of colors to four; one to two base colors, a trim color, and one to two accent colors. Examples of facades incorporating two to four colors are included in Figures 25 through 27.



Figure 25. Illustrating a three-color palette, with a base color, dark trim color, and a bright accent color.



Figure 26. Illustrating a two-color palette, with a dark base color and light trim.

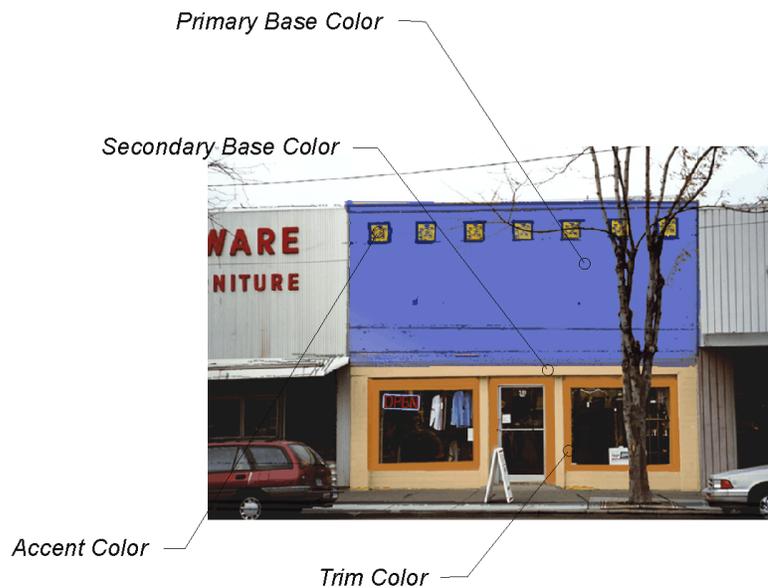


Figure 27. Illustrating a four color palette, with two contrasting base colors, a trim color and an accent color.

3.3.4 Facades along *pedestrian streets* must limit “neon” colors to 10%. Examples of neon colors are included in Figure 28.



Figure 28. Limiting neon colors to 10% of facades along pedestrian streets will help develop the desired visual identity for new commercial development in the Marina District.

3.3.5 Along *pedestrian streets*, prohibit free-standing pole signs.

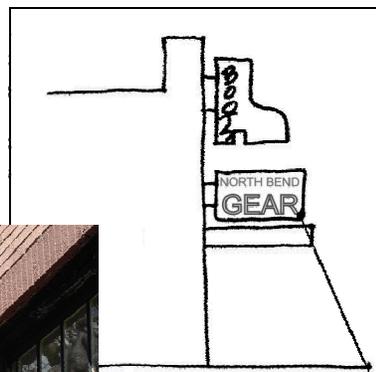
3.3.6 Along *pedestrian streets*, limit the size of building-mounted signs to 10% of the *façade*. Limit projecting (illustrated in Figure 30) signs to 12 square feet. Sign maintenance requirements are addressed in 15.16.120 of the City code.



Figure 29. Example of pole sign



Figure 30. Limiting the size of signage along pedestrian streets helps maintain a human scale and enhances the walking environment.



Hanging/projecting wall mounted signs may not exceed 12 square feet

## Design Guidelines

3.3.7 On *primary* and *secondary arterial routes*, untreated *blank walls* visible from a public street or pedestrian pathway are discouraged.

3.3.8 **All Sites:** The use of high quality building materials that add visual interest and detail, are durable and easily maintained, and that draw inspiration from early 20<sup>th</sup> Century construction typical to Westport are encouraged.

However, the use of overly ornate building details that make a building look fake or contrived are strongly discouraged. See Figure 31 below for an example.



*Figure 31. Overly ornate architectural styles and building details are discouraged.*

## 4. Landscaping

It is recommended that the City revise the existing landscaping requirement (Chapter 17.20A.060) to allow that:

- In the Marina District, lots over 20,000 sf may meet half (50%) of the required fifteen percent landscape / open space area with balconies, patios, rooftop decks that qualify as *pedestrian space* (Sub-chapter 2.3 in the design guidelines), or required view corridors (Ord. 1146).
- In the Marina District, reduce the requirement for lots 20,000 sf or less to 10% landscape / open space. Allow balconies, patios, rooftop decks that qualify as *pedestrian space*, and required view corridors (Ord. 1146) to meet the entire requirement.

The remaining open space / landscape area (on all lots but those 20,000 sf or less in the Marina District) must be landscaped according to the Site Landscaping section below.

### 4.1 Site Landscaping

#### Intent

- To enhance the aesthetic character of Westport's built environment.
- To define, break up, and screen parking areas.
- To reduce potential negative impacts on adjacent or neighboring uses.

#### Design Standards

- 4.1.1. **All sites:** On commercial lots, at least 50% of the open space requirement on a lot must be met with *landscaping* and walkways/pathways that access entrances and are no more than 12' wide. *Landscaping* must consist of a combination of trees, shrubs and living ground cover. The proposed landscaping should be consistent with the design intent and program of the building, the site, and use.
- 4.1.2. Along *primary* and *secondary arterial routes*, setbacks that do not include pedestrian space must provide landscaping as follows:
  - Trees at an average minimum rate of one tree per 30 lineal feet of street frontage.
  - Shrubs at the minimum rate of one per 20 square feet of landscaped area. Shrubs should have a mature height between three and four feet.
  - Groundcover shall be planted in sufficient quantities to provide at least 85 percent coverage of the landscaped area within three years of installation.



Figure 32. Examples of appropriate landscaping that mix a variety of trees, shrubs and ground cover.

## **Design Guidelines**

- 4.1.3 Developments are encouraged to utilize informal landscaping designs that use a variety of appropriate trees, shrubs, and ground covers. Formal landscaping arrangements may be appropriate in some cases to emphasize focal points or other special site features.
- 4.1.4 Use of native and drought-tolerant species is encouraged.
- 4.1.5. To prevent leaf litter from clogging gutters, deciduous or broadleaf evergreen trees should be planted at least 4 feet from curbs, especially in front of parking stalls. Where possible, coniferous trees should be planted at least 7 feet from curbs.
- 4. 1.6. Developments should plan for the mature size of trees and major shrubs to avoid interference with windows, decks or lighting.
- 4.1.7. Grass is acceptable as ground cover in landscaped areas, but not preferred for water conservation and maintenance purposes.

## 5. Definitions

*Adjoining* - Located next to; bordering or contiguous.

*Articulation* - The giving of emphasis to architectural elements (like windows, balconies, entries, etc.) that create a complementary pattern or rhythm dividing large buildings into smaller identifiable pieces.

*Blank walls* - A wall (including building façades and retaining walls) is considered a blank wall if:

- (a) A ground floor wall or portion of a ground floor wall over 6 feet in height has a horizontal length greater than 15 feet and does not include a window, door, building modulation or other architectural detailing; or
- (b) Any portion of a ground floor wall having a surface area of 400 square feet or greater does not include a window, door, building modulation or other architectural detailing.

*Cornice* - A horizontal molding projecting along the top of a wall, building, etc.

*Courtyard* - A landscaped space enclosed on at least three sides by a single structure.

*Façade* - Refers to building elevations facing an adjacent street and elevations that include a primary building entrance.

*Gabled roof* - A triangular wall enclosed by the sloping ends of a ridged roof.

*Human Scale* - The perceived size of a building relative to a human being. A building is considered to have “good” human scale if there is an expression of human activity or use that indicates the building's size. For example, traditionally sized doors, windows, and balconies are elements that respond to the size of the human body, so these elements in a building indicate a building's overall size.

*Landscaping* - The portion of a lot's open space consisting of a combination of trees, shrubs and living ground cover.

*Large-scale development* - Refers to development lots two or more acres in size.

*Major exterior remodels* - Include all remodels within a three year period whose value exceeds 50 percent of the value of the existing structure, as determined by the City of Westport valuation methods.

*Minor exterior remodels* - Include all remodels within a three year period with value of 50 percent of the building valuation or less.

*Modulation* - A stepping back or projecting forward of portions of a building facade within specified intervals of building width and depth, as a means of lessening the apparent bulk of a structure's continuous exterior walls

*Pedestrian-oriented façade* - A Ground floor facade that contains the following characteristics:

- Transparent window area or window displays along a minimum of 75 percent of the ground floor façade between a height of 2 feet to 8 feet above the ground.
- The primary building entry must be on this facade.
- Three off the following:
  - *Weather protection* (awning, marquee, canopy, e.g. that is at least 8' above the walking surface and projects at least 4-1/2 feet from the structure)
  - Wall mounted signage
  - Wall mounted lighting
  - Hanging/potted plants
  - Other wall mounted decorative elements that meet the Intent (subject to approval)

*Pedestrian-oriented space* - To qualify as “pedestrian-oriented space”, the following must be included:

- Visual and pedestrian access (including barrier-free access) to the abutting structures from the public right-of-way or a non-vehicular *courtyard*.
- Paved walking surfaces of either concrete or other approved unit paving.
- Pedestrian-scaled lighting (no more than 14' in height) at a level averaging 2-foot candles throughout the space. Lighting may be on-site or building-mounted.
- At least two feet of seating area (bench, ledge, etc.) or one individual seat per 60 square feet of plaza area or open space.

The following features are encouraged within pedestrian-oriented space:

- Pedestrian amenities such as a water feature, drinking fountain, tables, and/or distinctive paving or artwork.

The following features are discouraged within *pedestrian-oriented space*:

- Asphalt or gravel pavement.
- Adjacent unscreened parking lots.
- Adjacent chain link fences.
- Adjacent blank walls.
- Adjacent dumpsters or service areas.

*Pedestrian street* - Pedestrian streets are streets located in core commercial areas and evoke a “main street” feel. They include the following elements;

- Wide sidewalks
- Pedestrian amenities (pedestrian lighting, street furniture, e.g.)
- Street parking
- Buildings close to the sidewalk
- Pedestrian facades or pedestrian space

*Primary arterial routes* - Vehicular access routes that act as main arterials, usually with medium to large scale development and less pedestrian activity.

*Right-of-way* or (*ROW*) - The strip of land over which facilities such as highways, railroads, or power lines are built.

*Scale, architectural* - Means the perceived relative height and bulk of a building relative to that of neighboring buildings. A building's apparent height and bulk may be reduced by modulating facades.

*Secondary arterial routes* - Vehicular access routes with less vehicular activity and smaller-scale commercial development than *primary transportation routes*.

*Unique natural features* - May include steep slopes or other sensitive natural areas, a large boulder or boulders, a significant tree or trees, or other unique natural feature as determined by the City.

*Weather protection* - Architectural features such as an awning, marquee, or canopy that protect pedestrians from rain. To qualify as *weather protection*, the feature must be at least 8' above the walking surface and project at least 4-1/2 feet horizontally from the structure.